

ALABAMA WEST SUBDIVISION ADDITIONS

Engineering Comments: Verify that detention is not within flood plain; this is not allowed. Show all flood zones on the plat. Show minimum finished floor elevation on each lot affected by flood plain. It appears there is a drainage way on lot 13; if it conveys public water, a drainage easement is required. If there is a drainage way on-site conveying public water, any relocation of drainage way will require approval by City Engineering. It is the responsibility of the applicant to look up the site in the City of Mobile (COM) GIS system and verify if NWI wetlands are depicted on the site. If the COM GIS shows wetlands on the site, it is the responsibility of the applicant to confirm or deny the existence of wetlands on-site. If wetlands are present, they should be depicted on plans and/or plat, and no work/disturbance can be performed without a permit from the Corps of Engineers. Must comply with all storm water and flood control ordinances. Any work performed in the right of way will require a right of way permit.

Traffic Engineering Comments: Driveway numbers, size, location and design to be approved by Traffic Engineering and conform to AASHTO standards.

Urban Forestry Comments: Property to be developed in compliance with state and local laws that pertain to tree preservation and protection on both city and private properties (State Act 61-929 and City Code Chapters 57 and 64).

Fire Department Comments: All commercial projects shall comply with the 2003 International Fire Code and Appendices B through G as adopted by the City Of Mobile on July 6, 2004. All One- or Two-Family residential developments shall comply with Appendices B, C and D of the 2003 International Fire Code as adopted by the City of Mobile on July 6, 2004.

The plat illustrates the proposed 14.0± acre, 10 lot subdivision which is located at the Southeast corner of McVay Drive North and Halls Mill Road, and Northwest corner of McVay Drive North and Bolton Branch in City Council District 4. MAWSS has water and sewer services available, but a Capacity Assurance application for sewer service has not been applied for. MAWSS cannot guarantee sewer service until the Capacity application is approved by Volkert Engineering, inc.

A similar plat was before the Commission in November 2005, with one lot recorded in February 2006 and another in April 2006, a third was also recorded in April 2006 and then incorporated into another subdivision. As no other lots have been recorded, nor have any extensions been approved, the 2005 approval has expired.

The most significant difference between the previously approved plat and the plat now before the Commission is that Lot 9 has been reduced in size to accommodate a larger detention pond common area. Therefore, with the exception of the condition denying access to McLaughlin Drive North (Lot adjacent has already been recorded), all conditions of the previous approval

would still be appropriate. Unfortunately, the plat submitted does not reflect any of comments or conditions of the previous report and approval.

The following are excerpts from the previous report which remain applicable to this application:

The site fronts Halls Mill Road, a proposed collector street, and McVay Drive, a minor street. As a proposed collector street, the two-lane Halls Mill Road should have a minimum right-of-way of 70 feet to allow for future widening, thus sufficient right-of-way should be dedicated along the Halls Mill Road frontage to provide 35 feet from the centerline of Halls Mill Road. The right-of-way for the four-lane McVay Drive meets minimum requirements. It should also be noted that the property line at the corner of Halls Mill Road and McVay Drive should reflect the minimum required curb radius detailed in Section V.D.6. of the Subdivision Regulations.

Access management is a concern due to Halls Mill Road's status as a collector street, and due to the fact that McVay Drive connects Government Boulevard with Halls Mill Road, Navco Road and Dauphin Island Parkway. The site has extensive frontage onto both streets, thus there is potential for an extensive number of curb cuts, which consequently would reduce the traffic capacity of each roadway if the potential were fulfilled. Therefore, it would be appropriate for the applicant to consider an internal circulation system for the site in order to limit the number of curb cuts for the site. Furthermore, the consideration of an internal circulation system by the applicant may provide opportunities for innovative design. The applicant should coordinate with Urban Development (including Urban Forestry) and Traffic Engineering to determine the optimal layout for such a circulation system.

Curb cut limitations should be as follows: Lots 1-3 are limited to two shared curb-cuts onto Halls Mill Road; Lots 4-5 are limited to one shared curb-cut; Lot 6 is limited to two curbs-cuts; Lots 7-9 are limited to two shared curb-cuts; Lots 10-12 are limited to two shared curb-cuts; and Lot 13 is limited to two curb-cuts. The size, design and location of all curb-cuts to be approved by Urban Forestry and Traffic Engineering, and conform to AASHTO standards. Administrative PUD's will be required for the shared curb-cuts and internal circulation system.

The eastern portion of the site is bounded by the Bolton Branch of Dog River, and wetlands associated with the water body may occur on a portion of the site. The site, additionally, appears to be partially located in the "floodway" and "100-year" frequency flood zone as depicted on FEMA maps, and thus may be subject to flooding and hurricane-related storm surge. The presence of wetlands and floodplains indicate that the area may be environmentally sensitive; therefore, the approval of all applicable federal, state and local agencies would be required prior to the issuance of any permits.

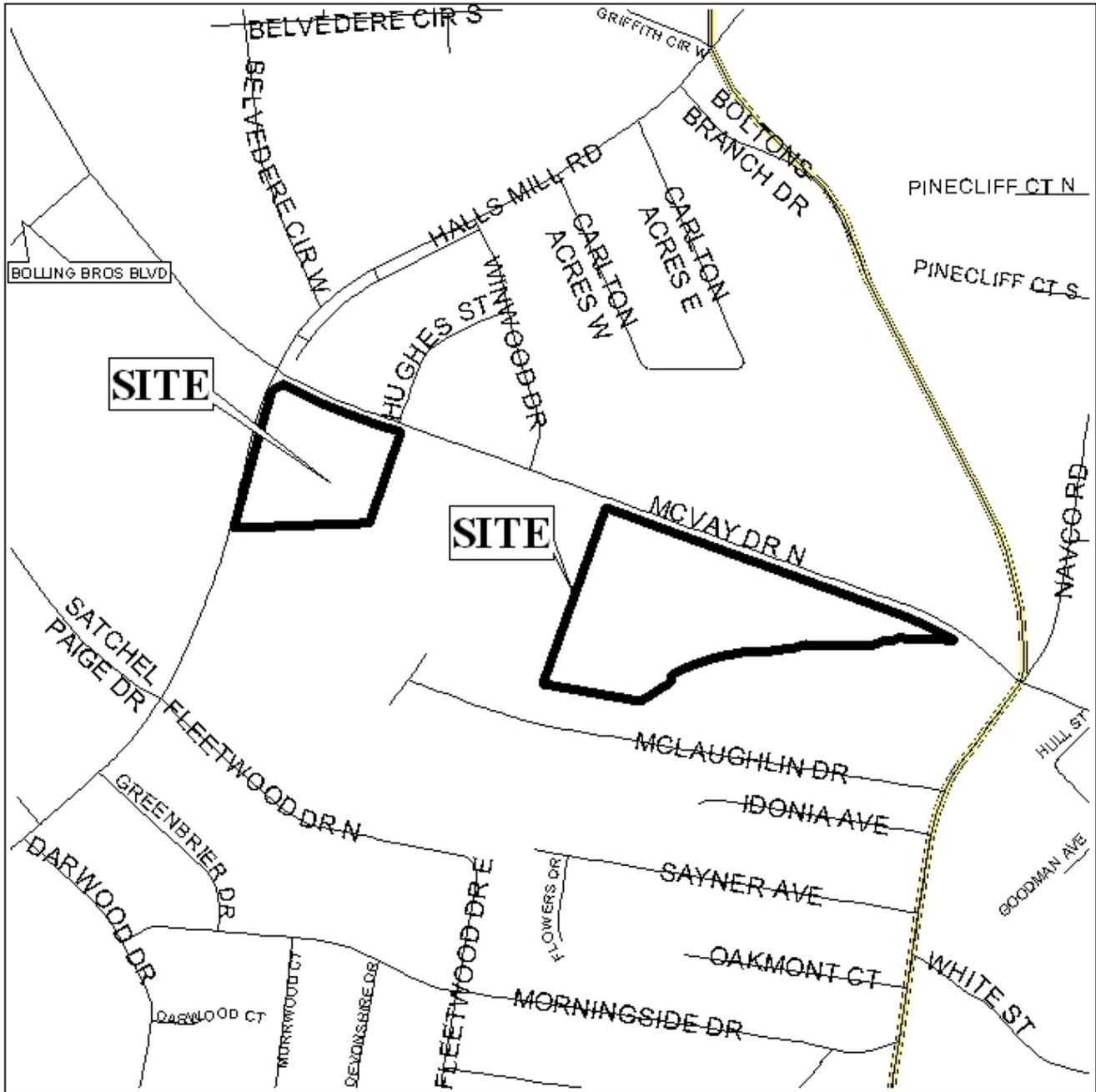
The preliminary plat does not indicate any means of access to the proposed detention pond, a common area for the subdivision. The plat should be revised to indicate an easement or dedicated access to the detention facility. Furthermore, a portion of the detention pond appears to be located in the 100-year floodplain. A note should be placed on the Final Plat, if approved, stating that maintenance of the detention pond common areas, and any other common areas, are the responsibility of the subdivision's property owners.

The 25-foot minimum building setback line, required in Section V.D.9. of the Subdivision Regulations, is not shown for the subdivision, but would be required on the Final Plat, if approved.

Based on the preceding, the plat is recommended for Tentative Approval subject to the following conditions:

1. dedication of adequate right-of-way to provide 35-feet from the centerline of Halls Mill Road;
2. revision of the plat to depict the property line radius requirements at the intersection of Halls Mill Road and McVay Drive, in compliance with Section V.D.6. of the Subdivision Regulations;
3. compliance with Engineering Comments (Verify that detention is not within flood plain; this is not allowed. Show all flood zones on the plat. Show minimum finished floor elevation on each lot affected by flood plain. It appears there is a drainage way on lot 13; if it conveys public water, a drainage easement is required. If there is a drainage way on-site conveying public water, any relocation of drainage way will require approval by City Engineering. It is the responsibility of the applicant to look up the site in the City of Mobile (COM) GIS system and verify if NWI wetlands are depicted on the site. If the COM GIS shows wetlands on the site, it is the responsibility of the applicant to confirm or deny the existence of wetlands on-site. If wetlands are present, they should be depicted on plans and/or plat, and no work/disturbance can be performed without a permit from the Corps of Engineers. Must comply with all storm water and flood control ordinances. Any work performed in the right of way will require a right of way permit.);
4. provision of 25-foot minimum building setback lines (reflecting any dedication along Halls Mill Road);
5. placement of a note on the final plat stating that Lots 1-3 are limited to two shared curb-cuts onto Halls Mill Road; Lots 4-5 are limited to one shared curb-cut; Lot 6 is limited to two curbs-cuts; Lots 7-9 are limited to two shared curb-cuts; Lots 10-12 are limited to two shared curb-cuts; and Lot 13 is limited to two curb-cuts. The size, design and location of all curb-cuts to be approved by Urban Forestry and Traffic Engineering, and conform to AASHTO standards;
6. depiction of an access easement or dedicated access to the detention pond;
7. placement of a note on the final plat stating that maintenance of the detention pond common areas, and any other common areas, are the responsibility of the subdivision's property owners; and
8. approval of all applicable federal, state and local agencies regarding the wetlands and floodplain issues prior to the issuance of any permits.

LOCATOR MAP



APPLICATION NUMBER 15 DATE September 6, 2007

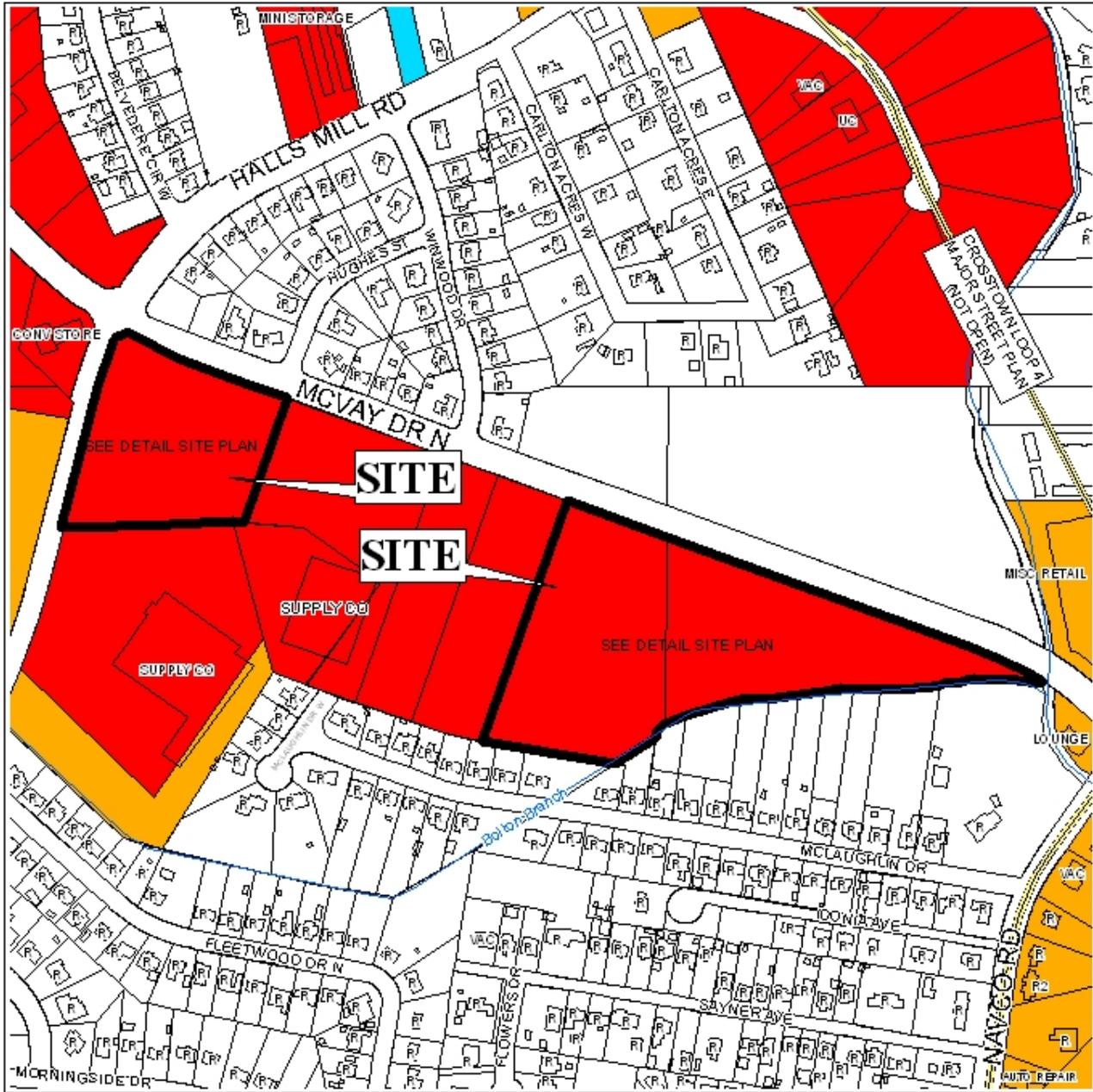
APPLICANT Alabama West Subdivision Additions

REQUEST Subdivision

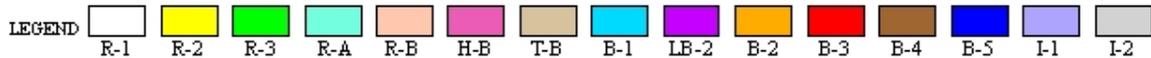


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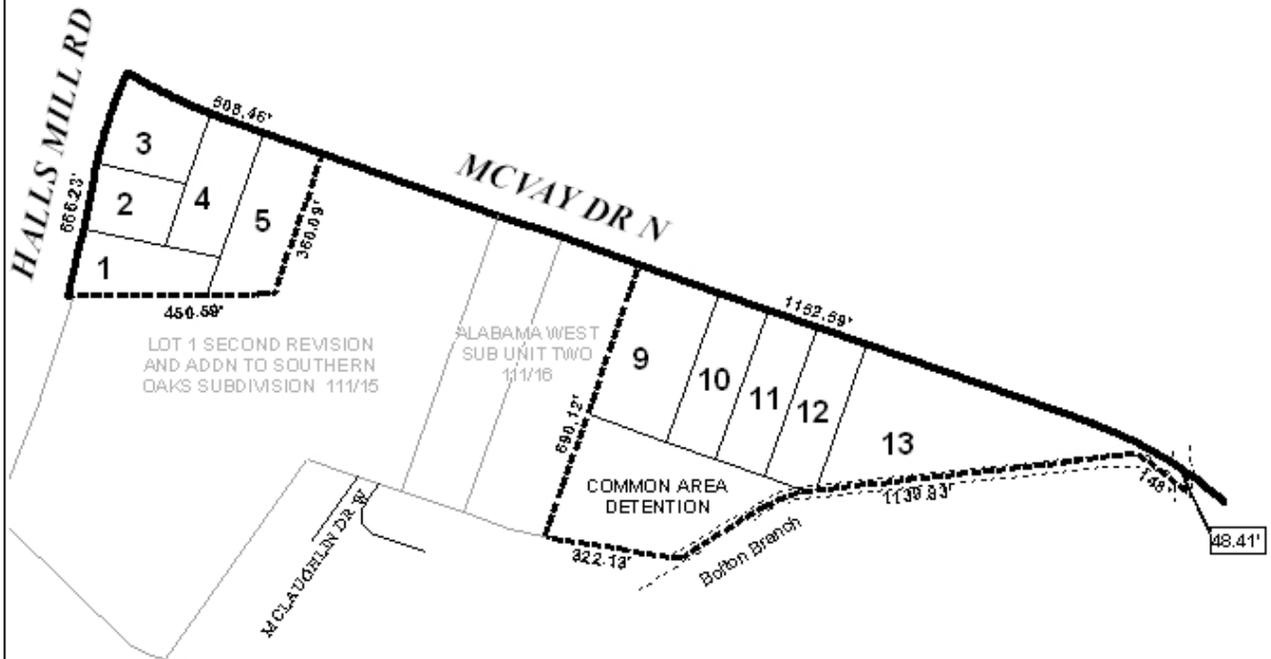
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APPLICATION NUMBER 15 DATE September 6, 2007



DETAIL SITE PLAN



APPLICATION NUMBER 15 DATE September 6, 2007

APPLICANT Alabama West Subdivision Additions

REQUEST Subdivision



NTS