

**SUBDIVISION &  
PLANNING APPROVAL STAFF REPORT**

**Date: April 5, 2018**

**NAME** Happy Campers RV Park Subdivision

**SUBDIVISION NAME** Happy Campers RV Park Subdivision

**LOCATION** North side of Coca Cola Road, 2/10± mile East of U.S. Highway 90 West and extending to the South side of Wiley Orr Road.

**CITY COUNCIL DISTRICT** District 4

**PRESENT ZONING** B-3, Community Business District.

**AREA OF PROPERTY** 1 Lots / 12.3± Acres

**CONTEMPLATED USE** Subdivision Approval to create one (1) legal lot of record and Planning Approval to allow a recreational vehicle park in a B-3, Community Business District.

**TIME SCHEDULE FOR DEVELOPMENT** Not specified.

**ENGINEERING**

**COMMENTS** **Subdivision:** FINAL PLAT COMMENTS (should be addressed prior to submitting the FINAL PLAT for review and/or signature by the City Engineer):

- A. Provide all of the required information on the SUBDIVISION PLAT (i.e. signature blocks, signatures, certification statements, written legal description, required notes, legend, scale, bearings and distances) that is required by the current Alabama State Board of Licensure for Professional Engineers and Land Surveyors.
- B. Provide the information required in NOTES #3.
- C. Show and label each and every Right-Of-Way and easement.
- D. Provide and label the monument set or found at each subdivision corner.
- E. Provide the Surveyor’s, Owner’s (notarized), Planning Commission, and Traffic Engineering signatures.
- F. Add a note that sidewalk is required to be constructed, and/or repaired, along the frontage of each lot, or parcel, at time of new development or construction, unless a sidewalk waiver is approved.
- G. Add a note to the plat stating that all proposed detention facilities, common areas, and wetlands shall be the responsibility of the Property Owner(s), and not the responsibility of the City of Mobile.

- H. Add a note to the plat stating that all easements shall remain in effect until vacated through the proper Vacation process.
- I. Provide a copy of the FINAL SUBDIVISION PLAT to the Engineering Dept. for review. No signatures are required on this drawing.
- J. After addressing all of the FINAL SUBDIVISION PLAT review comments by the Engineering Dept. provide the ORIGINAL (with all other signatures) and one (1) copy (signatures not required) of the revised Final Plat to the Engineering Department.

**Planning Approval:** ADD THE FOLLOWING NOTES TO THE SITE PLAN:

1. Any work performed in the existing ROW (right-of-way) such as driveways, sidewalks, utility connections, grading, drainage, irrigation, or landscaping will require a ROW permit from the City of Mobile Engineering Department (208-6070) and must comply with the City of Mobile Right-of-Way Construction and Administration Ordinance (Mobile City Code, Chapter 57, Article VIII).
2. A Land Disturbance Permit application shall be submitted for any proposed land disturbing activity with the property. A complete set of construction plans including, but not limited to, drainage, utilities, grading, storm water detention systems and paving will need to be included with the Land Disturbance permit. This Permit must be submitted, approved, and issued prior to beginning any of the construction work.
3. Any and all proposed land disturbing activity within the property will need to be submitted for review and be in conformance with Mobile City Code, Chapter 17, Storm Water Management and Flood Control; the City of Mobile, Alabama Flood Plain Management Plan (1984); and, the Rules For Erosion and Sedimentation Control and Storm Water Runoff Control.
4. The approval of all applicable federal, state, and local agencies (including all storm water runoff, wetland and floodplain requirements) will be required prior to the issuance of a Land Disturbance permit. The Owner/Developer is responsible for acquiring all of the necessary permits and approvals.
5. The proposed development must comply with all Engineering Department design requirements and Policy Letters.

### **TRAFFIC ENGINEERING**

#### **COMMENTS**

Site is limited to one curb cut to Coca-Cola Road, with size, location and design to be approved by Traffic Engineering and conform to AASHTO standards. Any new on-site parking, including ADA handicap spaces, shall meet the minimum standards as defined in Section 64-6 of the City's Zoning Ordinance.

### **URBAN FORESTRY**

#### **COMMENTS**

Property to be developed in compliance with state and local laws that pertain to tree preservation and protection on both city and private properties (State Act 2015-116 and City Code Chapters 57 and 64).

### **FIRE DEPARTMENT**

#### **COMMENTS**

All projects within the City Limits of Mobile shall comply with the requirements of the City of Mobile Fire Code Ordinance (2012 International Fire Code).

**MAWSS COMMENTS**

No comments.

**REMARKS**

The preliminary plat illustrates the proposed 1-lot, 12.3± acre subdivision located at the North side of Coca Cola Road, 2/10± mile East of U.S. Highway 90 West and extending to the South side of Wiley Orr Road, in Council District 4. The applicant is requesting Subdivision Approval to combine two (2) legal lots of record into one (1) legal lot of record, and Planning Approval to allow a recreational vehicle park in a B-3, Community Business District. The applicant states the property is served by public water and sanitary sewer systems.

Subdivision review examines the site with regard to promoting orderly development, protecting general health, safety and welfare, and ensuring that development is correlated with adjacent developments and public utilities and services; and, to ensure that the subdivision meets the minimum standards set forth in the Subdivision Regulations for lot size, road frontage, lot configuration, etc.

The review required for Planning Approval examines the applicant's location and site plan with regard to: a) transportation, parking and access; b) public utilities and facilities; c) traffic congestion and hazards; and, d) to determine if the proposal is in harmony with the orderly and appropriate development of the district.

It is very important to note that the Planning Approval review is site-plan specific; therefore *any* future changes to the site plan, as approved, by current or future applicants must be submitted for Planning Approval.

The site has been given a **Mixed Commercial Corridor** land use designation per the recently adopted Future Land Use Plan and Map. The Future Land Use Plan and Map complements and provides additional detail to the Development Framework Maps in the Map for Mobile, adopted by the Planning Commission at its November 5, 2015 meeting. This land use designation mostly applies to transportation corridors west of I-65 serving primarily the low-density (suburban) residential neighborhoods. It includes a wide variety of retail, services and entertainment uses.

The Mixed Commercial Corridor designation acknowledges existing commercial development that is spread along Mobile's transportation corridors in a conventional strip pattern, or concentrated into shorter segments of a corridor.

Over time, new development and re-development in Mixed Commercial Corridors is encouraged to: raise design quality; improve connectivity to surrounding neighborhoods; improve streetscapes; and, improve mobility and accessibility for all users of the corridor.

It should be noted that the Future Land Use Plan and Map components of the Map for Mobile Plan are meant to serve as a general guide, not a detailed lot and district plan. In many cases the designation on the new Future Land Use Map may match the existing use of land, but in others the designated land use may differ from what is on the ground today. As such, the Future Land Use Plan and Map allows the Planning Commission and City Council to consider individual cases based on additional information such as the classification request, the surrounding

development, the timing of the request, and the appropriateness and compatibility of the proposed use and, where applicable, the zoning classification.

The proposed lot is irregularly shaped, with 177.25'± of frontage along Coca Cola Road, a minor street without curb and gutter requiring a 60' right-of-way, and 315'± of frontage along Wiley Orr Road, which is also a minor street without curb and gutter requiring a 60' right-of-way. The preliminary plat illustrates sufficient right-of-way along Coca Cola Road, but not along Wiley Orr Road. The plat illustrates a 45' right-of-way along the frontage of this portion of Wiley Orr Road which is traditionally substandard for similar streets, but a 2-lot subdivision at the Northeast corner of U.S. Highway 90 West and Wiley Orr Road, approved by the Commission in 2010, only required dedication 25' from the centerline of Wiley Orr Road.

It should be noted that the widths of the portion of the lot along which it has frontages are large enough to be potentially re-subdivided and, in accordance with Section V.D.3. of the Subdivision Regulations are not exclusive of unusable land, thus contributing to a maximum depth more than 3.5 times the width of the lot at its potential building setback lines. Such a lot design may also be considered a "panhandle" lot, which is generally discouraged by Section V.D.1. of the Subdivision Regulations; the exceptions being the approval of lots located where varied and irregularly-shaped lot designs are common, and the informality of design is consistent with other lots in the vicinity. Lots with similar designs and width-to-depth ratios are within the vicinity of the subject site and were approved by the Planning Commission; perhaps the most recent being a 2-lot subdivision at its May 7, 2009 meeting. As such, approval of the request may be appropriate, but will require waivers of Sections V.D.1. and V.D.3. of the Subdivision Regulations to do so. Additionally, a note should be required on the Final Plat stating further re-subdivision of the lot will not be allowed until Wiley Orr Road is improved to City standards with regards to width and pavement.

Irrespective of its width-to-depth ratio and irregular design the proposed lot exceeds the minimum size requirements of the Subdivision Regulations for lots served by public water and sanitary sewer systems, and is appropriately labeled in square feet and acres on the preliminary plat. The 25' minimum building setback line is also appropriately labeled along all street frontages on the preliminary plat. This information should be retained on the Final Plat, if approved; or, the provision of a table on the Final Plat providing the same information may suffice.

With regards to access management, a note should be placed on the Final Plat stating the lot is limited to one (1) curb cut to Coca Cola Road, with any changes in its size, design, or location to be approved by Traffic Engineering and conform to AASHTO standards. An additional note should be placed on the Final Plat stating the lot is denied access to Wiley Orr Road until such time the road is improved in width to Engineering and Traffic Engineering standards.

Regarding Planning Approval, the applicant proposes the development of a recreational vehicle park with spaces for 91 recreational vehicles and utility pads for water, sewer, and electrical connections, and one (1) 2,400± square-foot "club house," to be completed in two (2) phases. Phase One concerns the development of the proposed club house; eight (8) off-street parking spaces for standard-sized vehicles, one (1) of which is van accessible, and all of which appear to

meet the minimum requirements for parking stall length and width; and 49 off-street recreational vehicle parking spaces with concrete patios and utility pads. Phase Two will concern the development of the remaining 42 off-street recreational vehicle parking spaces, also with concrete patios and utility pads.

The site plan illustrates both of the proposed phases of development of the recreational vehicle park. As mentioned, 91 off-street recreational vehicle parking spaces are proposed, the dimensions of which vary in two (2) ways: Type A, which will be 52.26' in length with 43.46' of frontage along a proposed drive aisle; and Type B, which will be 70' in length with two (2) 23.4'-wide frontages along two (2) proposed drive aisles. Each space will be equipped with a 2' X 9' concrete pad for utility connections, and a concrete patio – 10' X 10' for Type A spaces, and 8' X 10' for Type B spaces. Additionally, each space will be developed with a concrete apron providing access to a gravel-surfaced parking spot for each recreational vehicle, per drive aisle frontage. It should be noted, however, that gravel surfacing is not an approved surfacing material in a B-3, Community Business District. As such, revision of the site plan to illustrate recreational vehicle parking spaces surfaced in either concrete, asphalt or other approved surfacing material should be required; or, the applicant may seek Surface Variance Approval by the Board of Zoning Adjustment to allow gravel surfacing in a B-3, Community Business District.

Maneuverability on the site is facilitated by asphalt-paved drive aisles. Two-way maneuverability is proposed from Coca Cola Road continuing North, 376'±, before turning West and continuing 365'± to a one-way turnaround and ending at the Westernmost edge of the paved parking lot of the associated club house. One-way maneuverability is illustrated for the remainder of the site along three (3) drive aisles, two (2) of which will provide North-only access in the West and Center of the property, and only one (1) of which will provide South-only access in the East of the property. Each of the drive aisles is proposed to be at least 20' wide, thus exceeding the minimum width requirements for drive aisles providing one-way maneuverability; however, there may be concern with respect to only one (1) drive aisle providing access to a public street, especially in the event of an emergency. Nevertheless, maneuverability on the site will be directed by directional arrows on the pavement, as illustrated on the site plan. These illustrations should be retained on any revised site plan, if approved.

It should be noted that the aforementioned one-way turnaround at the club house is illustrated as having a diameter of 50'. While not a publicly-accessed turnaround, the 120' right-of-way diameter requirement of the Subdivision Regulations may prevail, especially with regards to facilitating the adequate maneuverability of emergency vehicles, should they be required. As such, the applicant should verify the minimum turnaround diameter with the Fire Department and adjust the plan if necessary.

Given the number of parking spaces proposed for standard-sized vehicles, a photometric plan is not required at this time. However, any site lighting should be in compliance with Section 64-6.A.3.c. of the Zoning Ordinance regarding parking lot lighting and illumination standards.

A dumpster is illustrated on the site plan, but is proposed to be located in such a way that trucks servicing the dumpster may block access to and from the public street, but may also have

difficulty accessing the dumpster. If approved, it may be appropriate to require revision of the site plan to illustrate a dumpster that will not affect accessibility to and from the site when being serviced by trucks. Nevertheless, a note should be placed on the site plan stating the dumpster will comply with Section 64-4.D.9. of the Zoning Ordinance regarding enclosure and sanitary sewer connection requirements. If the applicant chooses instead to utilize curbside trash pickup, then the plan should be revised to remove the proposed dumpster in lieu of a note stating curbside trash service will be utilized.

The site is currently undeveloped, thus development of the site requires full compliance with tree planting and landscape area ordinances. No trees or landscape area are illustrated on the site plan; therefore, if approved, a note should be placed on the site plan stating the site will comply with Section 64-4.E. of the Zoning Ordinance. A tree and landscape plan reflecting such compliance will be required prior to the approval of any land disturbing or construction activities.

No sidewalks are illustrated along either street frontage. The City adopted a Complete Streets policy in 2011 and, as such, revision of the site plan should be required to illustrate sidewalks along all street frontages; or, the applicant may seek approval of Sidewalk Waivers from the Planning Commission.

In coordination with the Subdivision, the 60' right-of-way along Coca Cola Road should be retained on the site plan. A 30' right-of-way is illustrated along Wiley Orr Road, which does not reflect the right-of-way along the same street illustrated on the preliminary plat. As such, revision of the site plan to illustrate a right-of-way along Wiley Orr Road equivalent to the Final Subdivision Plat should be required, if approved.

Also in coordination with the Subdivision, the size of the lot on which the Planning Approval development is proposed is indicated in square feet and acres in a table on the site plan. This information should be revised on the site plan to adjust for any required dedication, if approved.

The 25' minimum building setback line is illustrated along both street frontages on the site plan. This information should be retained on any revised site plan, if approved, adjusted for any required dedication.

If approved, a note should be placed on the site plan stating the lot is limited to one (1) curb cut to Coca Cola Road, with any changes in its size, design, or location to be approved by Traffic Engineering and conform to AASHTO standards. An additional note should be placed on the site plan stating the lot is denied access to Wiley Orr Road until such time the road is improved in width to Engineering and Traffic Engineering standards.

Finally, justification for the Planning Approval was not provided to staff by the applicant. No similar development and no requests for similar developments exist, or have been made within the vicinity of the subject site. The compatibility of the proposed development with the Future Land Use Plan and Map is not readily discernible, especially considering Mixed Commercial Corridors generally include varieties of retail, services, and entertainment. A recreational vehicle park may objectively be considered a service or a use similar to entertainment, but perhaps not

without additional information regarding how such uses are explicitly applicable to the proposed development. Its location along an established interstate corridor may, however, facilitate approval of the request since one objective of Mixed Commercial Corridor development is greater connectivity to surrounding neighborhoods.

It may be important to also point out that recreational vehicle parks are perhaps comparable to hotels/motels in that they offer an option for temporary lodging of travelers; and hotels/motels are uses allowed by right in B-3, Community Business Districts, several of which are adjacent or close to the subject site.

Nonetheless, the Planning Commission should carefully consider the individual merits of the request, as well as the appropriateness and compatibility of the proposed use as approval of a recreational vehicle park may introduce a use dissimilar to existing land uses within the vicinity.

### **RECOMMENDATION**

**Subdivision:** With waivers of Sections V.D.1. and V.D.3., the plat meets the minimum requirements of the Subdivision Regulations and is recommended for Tentative Approval, subject to the following conditions:

- 1) retention of the 60' right-of-way along Coca Cola Road;
- 2) retention of right-of-way dedication at least 25' from the centerline of Wiley Orr Road;
- 3) placement of a note on the Final Plat stating further re-subdivision of the lot will not be allowed until additional public street frontage is provided;
- 4) retention of the labels illustrating the lot size in both square feet and acres on the Final Plat, or provision of a table on the Final Plat providing the same information;
- 5) retention of the 25' minimum building setback line along all street frontages;
- 6) placement of a note on the Final Plat stating the lot is limited to one (1) curb cut to Coca Cola Road, with any changes in its size, location, or design to be approved by Traffic Engineering and conform to AASHTO standards;
- 7) placement of a note on the Final Plat stating the lot is denied access to Wiley Orr Road until such time the road is improved in width to Engineering and Traffic Engineering standards;
- 8) compliance with Engineering comments: (*FINAL PLAT COMMENTS (should be addressed prior to submitting the FINAL PLAT for review and/or signature by the City Engineer): A) Provide all of the required information on the SUBDIVISION PLAT (i.e. signature blocks, signatures, certification statements, written legal description, required notes, legend, scale, bearings and distances) that is required by the current Alabama State Board of Licensure for Professional Engineers and Land Surveyors. B) Provide the information required in NOTES #3. C) Show and label each and every Right-Of-Way and easement. D) Provide and label the monument set or found at each subdivision corner. E) Provide the Surveyor's, Owner's (notarized), Planning Commission, and Traffic Engineering signatures. F) Add a note that sidewalk is required to be constructed, and/or repaired, along the frontage of each lot, or parcel, at time of new development or construction, unless a sidewalk waiver is approved. G) Add a note to the plat stating that*

*all proposed detention facilities, common areas, and wetlands shall be the responsibility of the Property Owner(s), and not the responsibility of the City of Mobile. H) Add a note to the plat stating that all easements shall remain in effect until vacated through the proper Vacation process. I) Provide a copy of the FINAL SUBDIVISION PLAT to the Engineering Dept. for review. No signatures are required on this drawing. J) After addressing all of the FINAL SUBDIVISION PLAT review comments by the Engineering Dept. provide the ORIGINAL (with all other signatures) and one (1) copy (signatures not required) of the revised Final Plat to the Engineering Department.);*

- 9) compliance with Traffic Engineering comments: *(Site is limited to one curb cut to Coca-Cola Road, with size, location and design to be approved by Traffic Engineering and conform to AASHTO standards. Any new on-site parking, including ADA handicap spaces, shall meet the minimum standards as defined in Section 64-6 of the City's Zoning Ordinance.);*
- 10) compliance with Urban Forestry comments: *(Property to be developed in compliance with state and local laws that pertain to tree preservation and protection on both city and private properties (State Act 2015-116 and City Code Chapters 57 and 64).);*
- 11) subject to the Fire Department comments: *(All projects within the City Limits of Mobile shall comply with the requirements of the City of Mobile Fire Code Ordinance (2012 International Fire Code).);*
- 12) provision of two copies of the Planning Approval site plan to the Planning and Zoning Department prior to the signing of the Final Plat;
- 13) full compliance with all applicable Codes and Ordinances; and,
- 14) completion of the Subdivision process prior to any requests for land disturbing or construction activities.

**Planning Approval:** Based upon the preceding, staff recommends to the Planning Commission the following findings of fact for Approval of the Planning Approval:

- a) the proposal will be appropriate with regard to transportation, parking and access because it will facilitate recreational vehicles along a major interstate corridor, provide parking of those vehicles as well as personal vehicles via an associated club house, and guarantee access to a public street;
- b) the proposal will be appropriate with regard to the use of public utilities and facilities by ensuring access to public water and sanitary sewer systems, waste disposal, and fire and police protection via dedicated accessibility to a public right-of-way;
- c) the proposal will not cause undue traffic congestion traffic or create a traffic hazard by limiting the site to one (1) access to a public street until such time an additional street can be improved to City standards, and by containing excess maneuverability to the site; and,
- d) the proposal will be in harmony with the orderly and appropriate development of the district in which the use is located because it is a comparable use to hotels/motels in that it will offer an option for temporary lodging of travelers, which is a use allowed by right in the underlying zoning district.

The approval is subject to the following conditions:

- 1) revision of the site plan to illustrate recreational vehicle parking spaces surfaced in either concrete, asphalt or other approved surfacing material, or Surface Variance Approval by the Board of Zoning Adjustment to allow gravel surfacing in a B-3, Community Business District;
- 2) retention of the proposed directional arrows on any revised site plan to facilitate maneuverability on the site;
- 3) coordination with the Fire Department to ensure adequate site circulation and revise the site plan, as necessary;
- 4) placement of a note on the revised site plan stating the site will comply Sections 64-6.A.3.c. the Zoning Ordinance regarding parking lot lighting and illumination standards;
- 5) placement of a note on the revised site plan stating the proposed dumpster will comply with Section 64-4.D.9. of the Zoning Ordinance regarding enclosure and sanitary sewer requirements, or revision of the plan to remove the dumpster in lieu of a note stating curbside trash service will be utilized;
- 6) placement of a note on the revised site plan stating the site will comply with Section 64-4.E. of the Zoning Ordinance regarding tree planting and landscape requirements;
- 7) revision of the site plan to illustrate sidewalks along both street frontages, or Sidewalk Waiver Approvals by the Planning Commission;
- 8) retention of the 60' right-of-way along Coca Cola Road;
- 9) revision of the site plan to illustrate a right-of-way along Wiley Orr Road equivalent to the Final Subdivision Plat;
- 10) retention of the lot size in square feet and acres on the site, adjusted for right-of-way dedication;
- 11) retention of the 25' minimum building setback line along all street plans on the revised site plan;
- 12) placement of a note on the site plan stating the lot is limited to one (1) curb cut to Coca Cola Road, with any changes in its size, location, or design to be approved by Traffic Engineering and conform to AASHTO standards;
- 13) placement of a note on the site plan stating the lot is denied access to Wiley Orr Road until such time the road is improved in width to Engineering and Traffic Engineering standards;
- 14) compliance with Engineering comments: *(ADD THE FOLLOWING NOTES TO THE SITE PLAN: 1) Any work performed in the existing ROW (right-of-way) such as driveways, sidewalks, utility connections, grading, drainage, irrigation, or landscaping will require a ROW permit from the City of Mobile Engineering Department (208-6070) and must comply with the City of Mobile Right-of-Way Construction and Administration Ordinance (Mobile City Code, Chapter 57, Article VIII). 2) A Land Disturbance Permit application shall be submitted for any proposed land disturbing activity with the property. A complete set of construction plans including, but not limited to, drainage, utilities, grading, storm water detention systems and paving will need to be included with the Land Disturbance permit. This Permit must be submitted, approved, and issued prior to beginning any of the construction work. 3) Any and all proposed land disturbing activity within the property will need to be submitted for review and be in conformance with Mobile City Code, Chapter 17, Storm Water Management and Flood Control); the*

*City of Mobile, Alabama Flood Plain Management Plan (1984); and, the Rules For Erosion and Sedimentation Control and Storm Water Runoff Control. 4) The approval of all applicable federal, state, and local agencies (including all storm water runoff, wetland and floodplain requirements) will be required prior to the issuance of a Land Disturbance permit. The Owner/Developer is responsible for acquiring all of the necessary permits and approvals. 5) The proposed development must comply with all Engineering Department design requirements and Policy Letters.);*

- 15) compliance with Traffic Engineering comments: *(Site is limited to one curb cut to Coca-Cola Road, with size, location and design to be approved by Traffic Engineering and conform to AASHTO standards. Any new on-site parking, including ADA handicap spaces, shall meet the minimum standards as defined in Section 64-6 of the City's Zoning Ordinance.);*
- 16) compliance with Urban Forestry comments: *(Property to be developed in compliance with state and local laws that pertain to tree preservation and protection on both city and private properties (State Act 2015-116 and City Code Chapters 57 and 64).);*
- 17) compliance with Fire Department comments: *(All projects within the City Limits of Mobile shall comply with the requirements of the City of Mobile Fire Code Ordinance (2012 International Fire Code).);*
- 18) provision of two copies of the revised Planning Approval site plan to the Planning and Zoning Department prior to the signing of the Final Plat; and,
- 19) full compliance with all applicable Codes and Ordinances.

# LOCATOR MAP



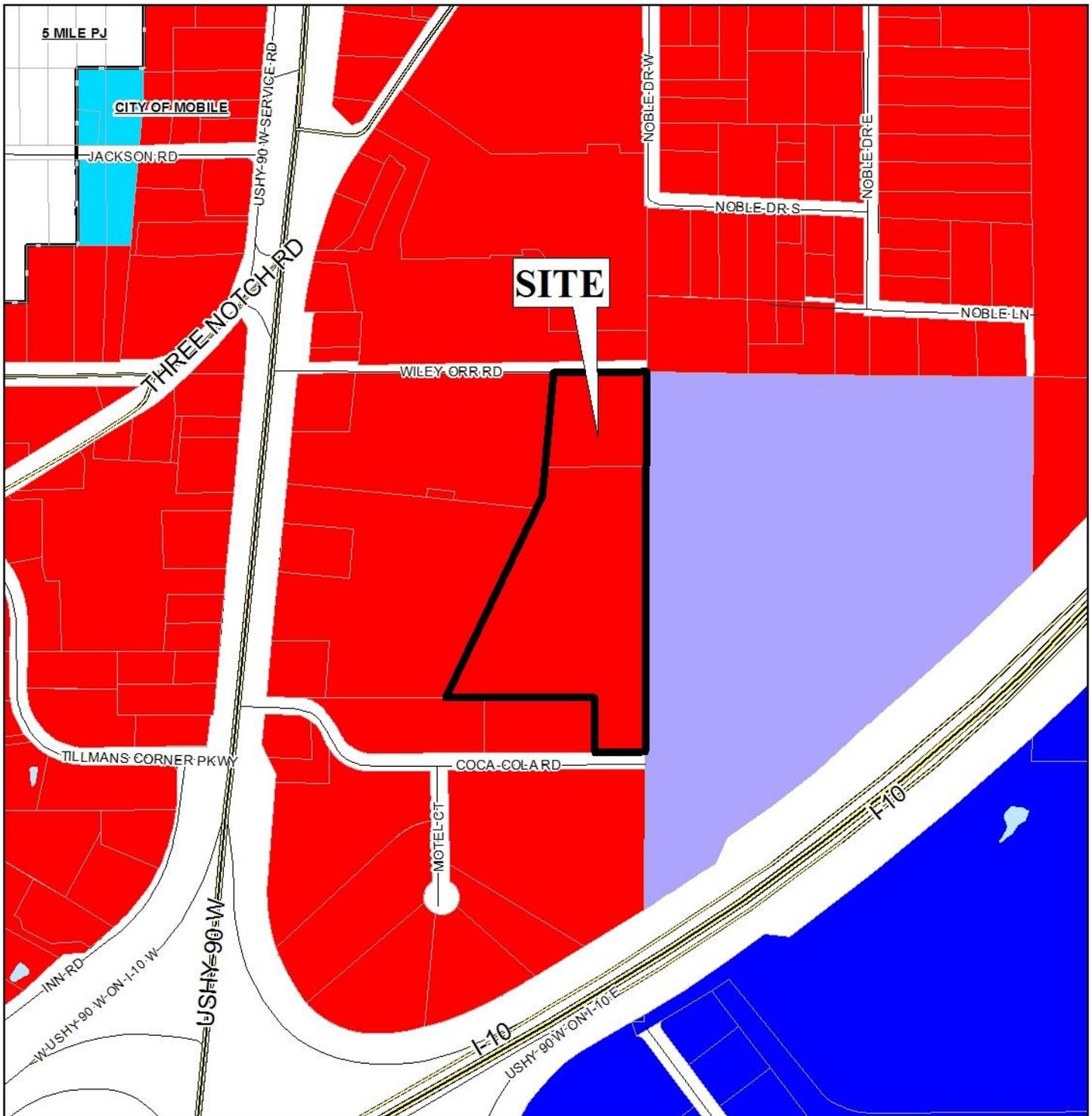
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APPLICANT Happy Campers RV Park Subdivision

REQUEST Subdivision, Planning Approval



# LOCATOR ZONING MAP



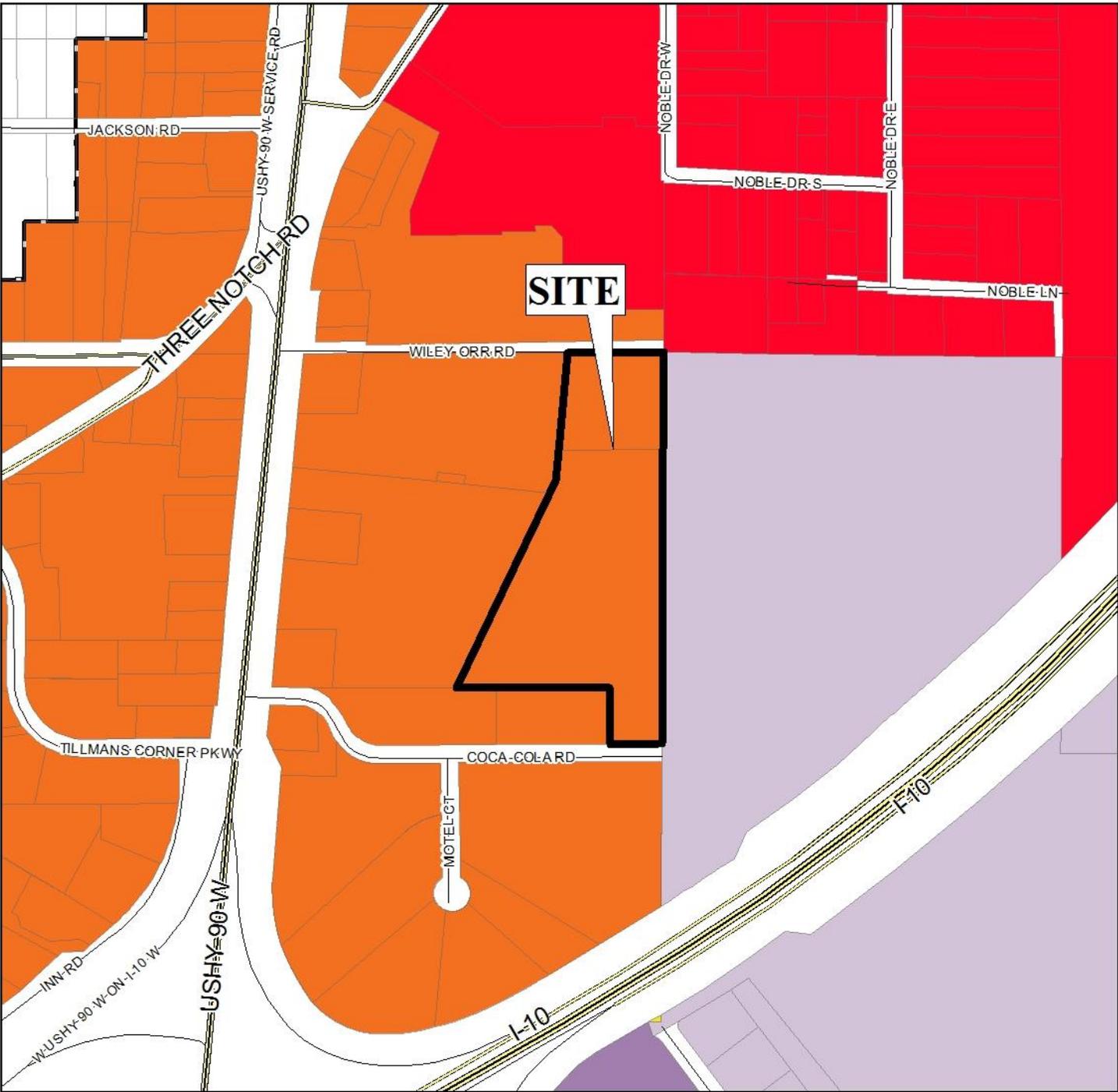
APPLICATION NUMBER 13 DATE April 5, 2018

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REQUEST Subdivision, Planning Approval



# FLUM LOCATOR MAP



APPLICATION NUMBER 13 DATE April 5, 2018

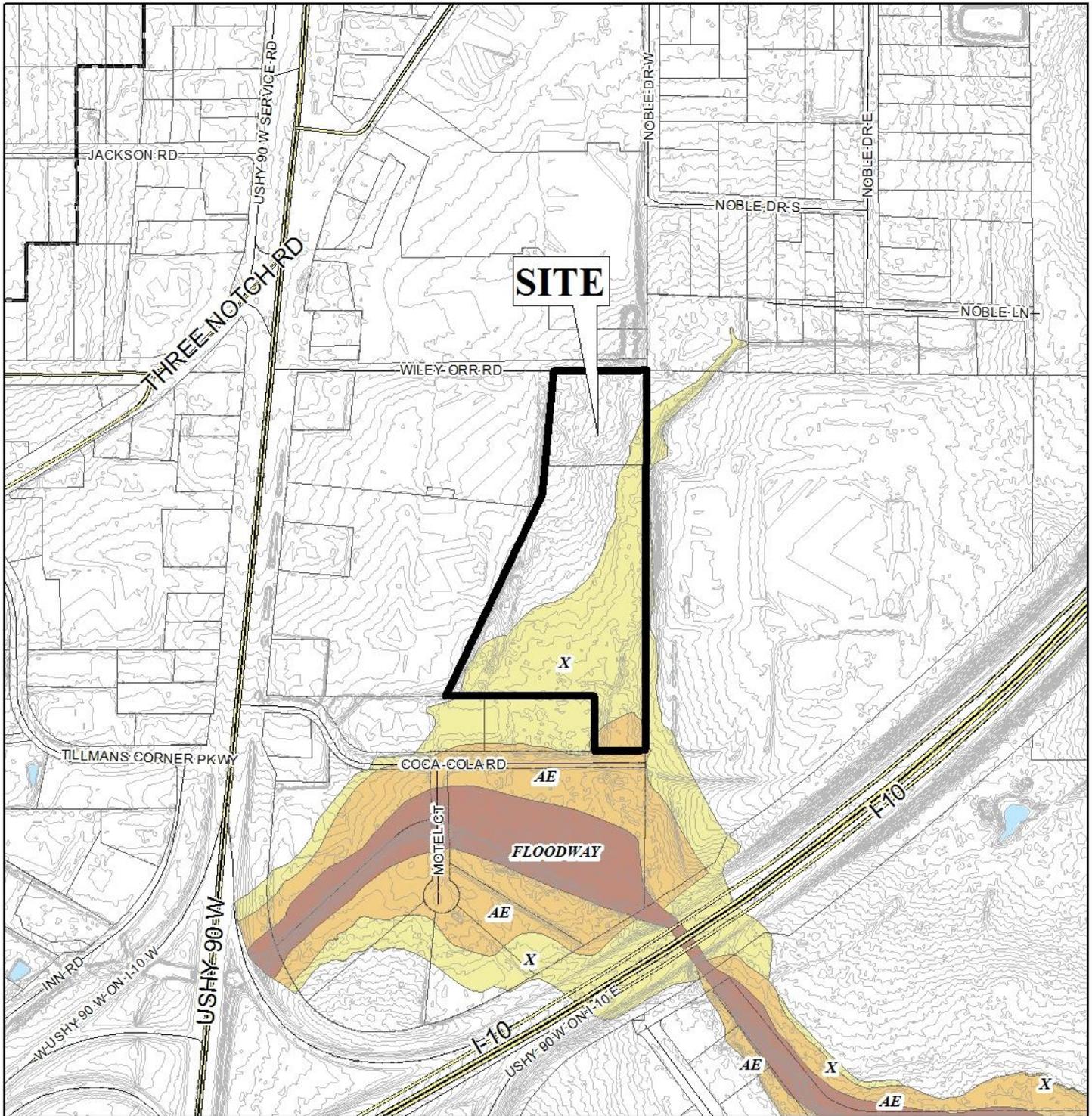
APPLICANT Happy Campers RV Park Subdivision

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|---|---|---|--|
| <span style="display: inline-block; width: 15px; height: 10px; background-color: yellow; border: 1px solid black;"></span> Low Density Residential    | <span style="display: inline-block; width: 15px; height: 10px; background-color: #800000; border: 1px solid black;"></span> Neighborhood Center - Traditional | <span style="display: inline-block; width: 15px; height: 10px; background-color: #A52A2A; border: 1px solid black;"></span> Downtown Waterfront | <span style="display: inline-block; width: 15px; height: 10px; background-color: #9ACD32; border: 1px solid black;"></span> Parks & Open Space |
| <span style="display: inline-block; width: 15px; height: 10px; background-color: #FFD700; border: 1px solid black;"></span> Mixed Density Residential | <span style="display: inline-block; width: 15px; height: 10px; background-color: #0000FF; border: 1px solid black;"></span> Neighborhood Center - Suburban    | <span style="display: inline-block; width: 15px; height: 10px; background-color: #E6E6FA; border: 1px solid black;"></span> Light Industry      | <span style="display: inline-block; width: 15px; height: 10px; background-color: #008080; border: 1px solid black;"></span> Water Dependent    |
| <span style="display: inline-block; width: 15px; height: 10px; background-color: #FF00FF; border: 1px solid black;"></span> Downtown                  | <span style="display: inline-block; width: 15px; height: 10px; background-color: #FF69B4; border: 1px solid black;"></span> Traditional Corridor              | <span style="display: inline-block; width: 15px; height: 10px; background-color: #800080; border: 1px solid black;"></span> Heavy Industry      |  |
| <span style="display: inline-block; width: 15px; height: 10px; background-color: #FF0000; border: 1px solid black;"></span> District Center           | <span style="display: inline-block; width: 15px; height: 10px; background-color: #FF8C00; border: 1px solid black;"></span> Mixed Commercial Corridor         | <span style="display: inline-block; width: 15px; height: 10px; background-color: #00BFFF; border: 1px solid black;"></span> Institutional       |  |



# ENVIRONMENTAL LOCATOR MAP



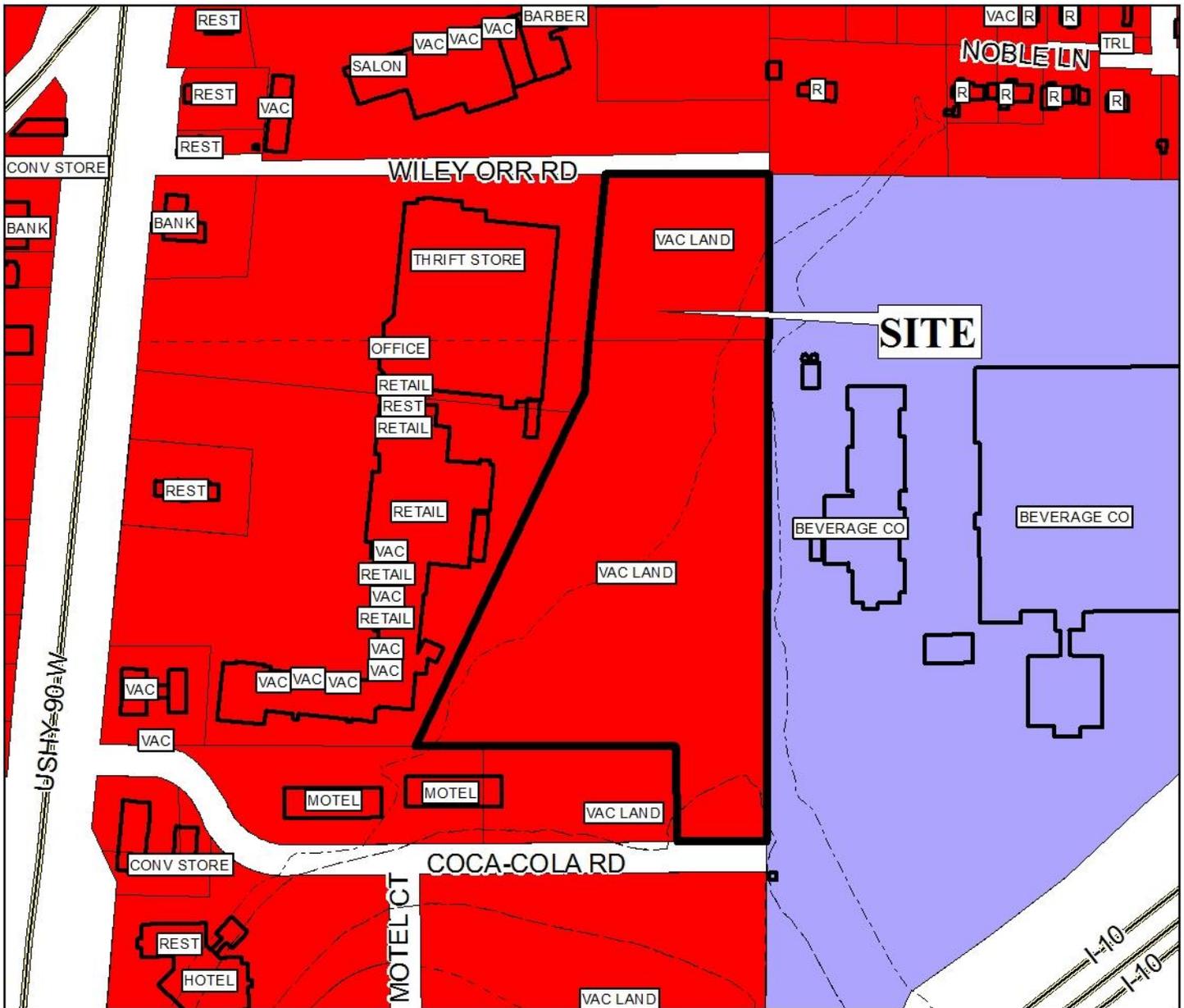
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# PLANNING COMMISSION VICINITY MAP - EXISTING ZONING



The site is surrounded by commercial units. Residential units are located to the northeast.

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REQUEST       Subdivision, Planning Approval      

 R-A	 R-3	 T-B	 B-2	 B-5	 MUN	 SD-WH	 T5.1
 R-1	 R-B	 B-1	 B-3	 I-1	 OPEN	 T3	 T5.2
 R-2	 H-B	 LB-2	 B-4	 I-2	 SD	 T4	 T6



# PLANNING COMMISSION VICINITY MAP - EXISTING AERIAL



The site is surrounded by commercial units. Residential units are located to the northeast.

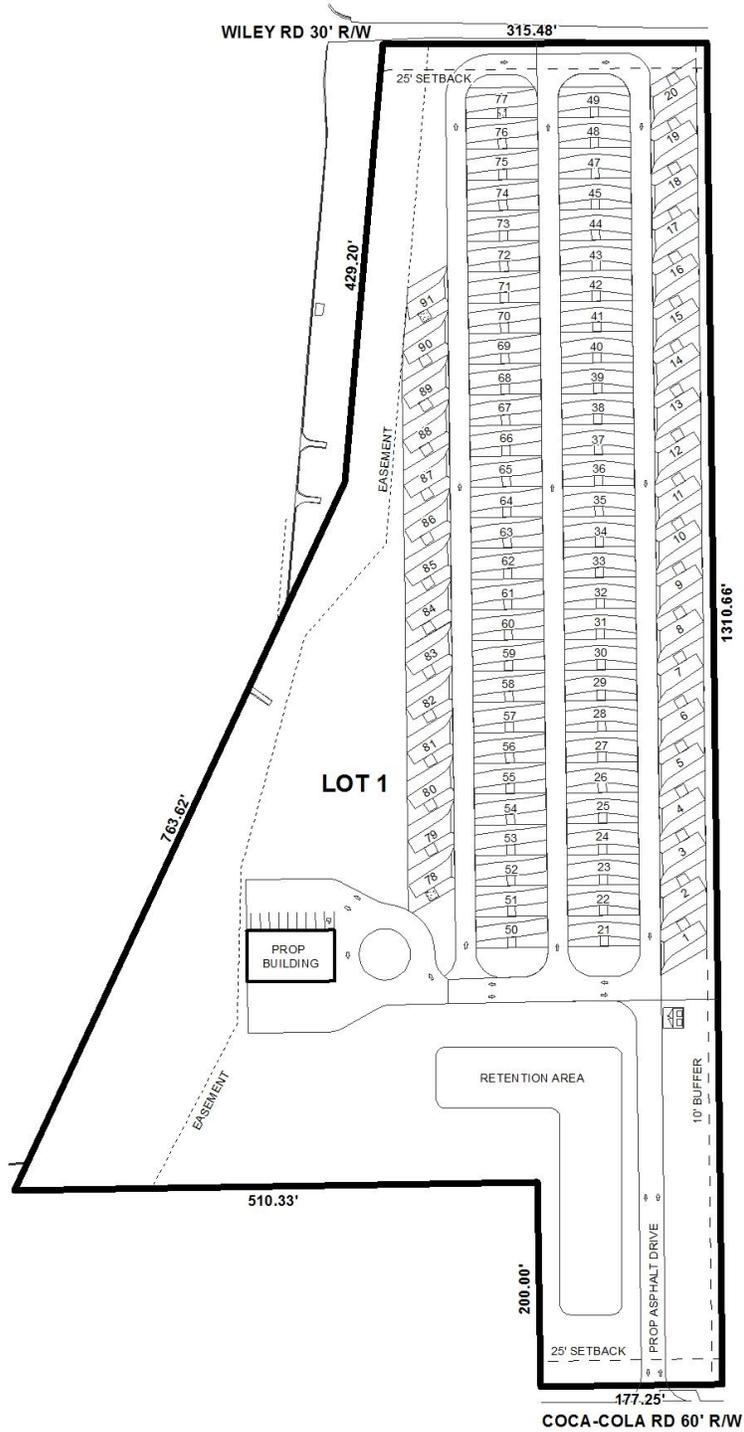
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# SITE PLAN



The site plan illustrates the easement, setbacks, proposed building, proposed campsites, and proposed retention area.

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NTS

